

ARTICLE 23

PARKING, LOADING AND ACCESS

490-2800 Legislative findings

The Common Council makes the following legislative findings:

- (1) The design of parking areas and site access is critically important to the economic viability of commercial areas, pedestrian and driver safety, the efficient and safe operation of adjoining streets, and community image and livability.
- (2) Standards are needed to establish the minimum and maximum number of parking spaces that are needed to serve various land uses.
- (3) Excessively large parking lots increase the cost of development, create an unhealthy built environment, contribute to the heat island effect associated with urban areas, and decrease the infiltration of storm water into the ground.
- (4) Shared parking can reduce parking facility costs (including aesthetic and environmental impacts), allow greater flexibility in facility location and site design, and encourage more efficient land use.
- (5) Parking lots and their access represent a vital connection between the local transportation network and land uses.
- (6) Incorrectly designed parking lots and site access can have negative impacts on the site, the adjacent and nearby public roadways, and the image of the business community.

490-2801 Purpose

This article promotes the public health, safety, and general welfare and is intended to:

- (1) increase the safety and capacity of public streets by requiring off-street parking and off-street loading facilities,
- (2) minimize adverse effects of off-street parking and off-street loading facilities on adjacent properties and surrounding neighborhoods,
- (3) lessen congestion and prevent the overtaxing of public roads by regulating the location and capacity of off-street parking and off-street loading facilities,
- (4) maintain and enhance a safe and efficient transportation system,
- (5) minimize the occurrences of motor vehicles backing into public roads, and
- (6) minimize impervious surfaces.

490-2802 General design principles

Parking areas, loading berths, driveways, and access must be designed based on the following principles:

- (1) Provide continuous flow of traffic through the parking area.
- (2) Allow safe movement of pedestrians from parking areas to buildings.
- (3) Avoid conflicts between vehicular, bicycle, and pedestrian traffic. Pedestrian circulation takes precedence over vehicular circulation.
- (4) Ensure that site facilities and amenities are accessible to people with disabilities as required by this article and the Americans with Disabilities Act (ADA).
- (5) Allow for appropriate landscaping of parking areas without conflicting with outdoor lighting.

- (6) Ensure that emergency service vehicles are able to travel through parking areas, including fire trucks.
- (7) Minimize impervious surfaces.
- (8) Allow for the logical expansion of parking areas to accommodate different land uses or an expansion of an existing use.
- (9) Ensure that the stormwater generated on the site is accommodated consistent with City requirements.
- (10) Queuing of vehicles must be designed to prevent stacking of vehicles onto a public right-of-way.
- (11) Ingress and egress of parking spaces must occur on private property and not cross into the public right of way (sidewalks or streets) to gain access to parking spaces.

490-2803 General applicability

The requirements and standards for off-street parking apply to new site development, an expansion or modification of an existing parking area, and a change in use, subject to the standards below.

- (1) When an existing building, structure, or use of land changes to a new use, additional on-site parking spaces must be constructed if the new use requires a minimum amount of on-site parking spaces more than already exists on the subject property.
- (2) When a building or structure is expanded, additional parking spaces must be constructed if the expansion requires a minimum amount of on-site parking spaces more than already exists on the subject property.
- (3) When a building or structure was erected prior to [REDACTED], 2026, additional minimum parking spaces or loading facilities are mandatory only in the amount by which the requirements of the new use or occupancy exceed those for the existing or prior use or occupancy.
- (4) When site constraints and/or other standards of this chapter do not allow proper space to provide for the minimum amount of required on-site parking spaces for a change in use of a building, structure, or land as stated above, the Plan Commission may, in its discretion, grant a special exception to the requirement of constructing additional parking spaces. The applicant requesting a change in use must demonstrate how site constraints and/or other standards of this chapter do not allow construction of additional on-site parking spaces, and must provide justification that there are sufficient off-site parking spaces available for overflow parking, such as street parking or shared parking on neighboring or nearby properties.

490-2804 General standards for off-street parking areas

- A. Proximity of parking to principal use.** Parking spaces must be located on the same lot with the principal use, except when shared parking is allowed.
- B. Use of parking spaces.** Off-street parking areas for non-residential uses must be for occupants, employees, visitors, and patrons. The storage of merchandise, supplies, motor vehicles for sale, or the repair of vehicles in a parking area is prohibited, unless otherwise allowed in this chapter. In addition, overnight camping (e.g., RVs) in a parking area is prohibited.
- C. Surfacing.** An off-street parking area (i.e., spaces and aisles) must be hard surfaced with concrete, asphalt, pavers, or other durable, all-weather pavement material approved by the Zoning Administrator.
- D. Curb.** An off-street parking area with 20' or parking spaces must be constructed with a concrete curb surrounding the vehicle use areas (both driveways and parking areas). If an existing

parking lot ; except the Zoning Administrator may waive this requirement of curb and gutter an existing parking lot is expanded that does not have a curb. Gaps in the curbing may occur to facilitate property parking lot drainage. (Staff to verify)

- E. **Marking of parking spaces.** Parking spaces must be clearly marked if there are more than 3 parking spaces.
- F. **Drainage.** An off-street parking area must be properly graded for drainage consistent with standards adopted by the city, and as approved by the city engineer.
- G. **Snow storage.** Required parking spaces and access aisles may not be used for snow storage. Areas used for snow storage must be clearly depicted on the approved site plan if snow will be stored on site.
- H. **Features within vision triangle.** All features and improvements within a vision triangle must be consistent with the standards in § 490- .
- I. **Landscaping.** Landscaping for an off-street parking area must be provided consistent with the requirements in Article 22.
- J. **Screening.** An off-street parking area with 4 or more parking spaces must provide screening as described in Article 22.
- K. **Outdoor lighting.** Outdoor lighting within a parking area must be provided consistent with the requirements in Article 24.
- L. **Signage.** Signage related to off-street parking and on-site traffic circulation must comply with the requirements in Article 25.

Exhibit 23-1. Green infrastructure next to a parking lot



490-2805 Placement on a lot

- A. **Generally.** Parking areas may be located in any yard provided applicable pavement setback requirements are met.
- B. **Restriction in some zoning districts.** The number of parking spaces located in the front yard may be restricted in some zoning districts as more fully set forth in § 490- . (Staff- This needs to be updated based on input on other parts of the zoning code.)

490-2806 Timing

Parking spaces and aisles, and curb and gutter, if required, must be installed before the granting of occupancy for the use or uses for which the vehicle use areas are provided. If weather conditions during the late fall to early spring seasons do not allow installation of the permanent surface and curb and gutter, the Building Inspector may grant temporary occupancy, provided a solid surface of gravel is provided and the property owner and City enter into a written agreement, with a financial guarantee, that requires completion no later than June 1 or as specified in the agreement.

490-2807 Number of parking spaces

- A. **Minimum number of spaces.** Off-street parking spaces must be provided in the number specified in Appendix B, except as follows:
 - (1) Land uses located in the Downtown (C-4) zoning district are not required to provide off-street parking.

- (2) When bicycle parking is provided consistent with this article, bicycle parking spaces may be used to satisfy the number of required parking spaces up to a maximum of 4 percent provided the number of required parking spaces is 25 or more. For example, if the parking standards as applied to a project call for 100 vehicle parking spaces, no more than 4 bicycle parking spaces may be substituted (96 vehicle parking spaces and 4 bicycle parking spaces).
 - (3) When motorcycle parking is provided consistent with this article, motorcycle parking spaces may be used to satisfy the required parking spaces up to a maximum of 4 percent provided the number of required parking spaces is 25 or more.
- B.** Upon a written petition, the Zoning Administrator may grant an administrative adjustment to allow the use of a lesser parking standard for a particular land use pursuant to the procedures and requirements in Article 5, provided the reduction does not exceed 40 percent of the required amount. If a reduction is requested, the applicant must provide technical documentation during the site plan review process to indicate, to the satisfaction of the Zoning Administrator, that actual off-street parking demand for that particular use is less than the required amount. If approved, an area on the property must be held in reserve for the potential future development of paved off-street parking to meet the full requirements. This reserved area must be shown and noted on the site plan, maintained as open space, and developed with paved off-street parking spaces when the Zoning Administrator determines that such off-street parking is necessary due to parking demand on the property which exceeds original expectations. The reserved area does not count as part of any required greenspace area. The Zoning Administrator may require the property owner to provide a financial guarantee as set forth in Article 4 in the amount of 125 percent of the anticipated cost of installing the required number of parking spaces.
- C. Maximum number of spaces.** For land uses located in a commercial or industrial zoning district, the number of parking spaces provided in a ground surface parking lot may not exceed the number of minimum parking spaces by more than 15 percent, except that the Plan Commission may allow more parking spaces above that threshold as a special exception pursuant to the procedures and requirements in Article 5 provided the commission determines that additional spaces are needed for that particular use or location. There is no maximum limit on the number of parking spaces when located in a parking garage or similar structure.
- D. Mixed-use requirements.** For mixed uses, the total requirements for off-street parking spaces is the sum of the requirements for the various land uses. Off-street parking facilities for one use may not be considered as providing parking facilities for any other use except when considered shared parking as allowed in this article.
- E. Compact cars and motorcycles.** Up to 10 percent of the required number of parking spaces may be sized for compact cars and/or motorcycles. Such parking spaces must be designated by a sign or other means approved by the Building Inspector.

490-2808 Shared parking

- A. Generally.** The Zoning Administrator, in their discretion, may allow shared parking consistent with this section during the site plan review process provided the applicant clearly shows the timeframe for peak parking demand varies between the different land uses that are subject to the shared parking arrangement. By way of example, Exhibit 23-2 shows different land uses that could potentially share parking spaces.
- B. Maximum.** The maximum amount of required parking spaces that can be shared is 50 percent of the lesser number of required spaces. For example, if the first land use requires 100 parking spaces and the second land use requires 50 spaces, the maximum number of shared spaces is 25.

- C. Minimum requirements.** Shared parking must meet the standards listed below.
 - (1) The area of the parking lot that is designated for shared use, must be located on an adjoining parcel and within 500 feet of the building or use requiring the parking.
 - (2) Directional signage is provided where appropriate and allowed.
 - (3) Pedestrian routes are direct, clear, and safe.
- D. Shared-use agreement.** The parties involved in the joint use of off-street parking facilities must document their agreement by a legal instrument approved by the city attorney as to form and content. Such instrument, when approved as conforming to the provisions of this part, must be recorded in the register of deeds office for Ozaukee County, and a copy filed with the Zoning Administrator.
- E. Effect on other standards.** Driveways, parking areas, and parking spaces of adjacent properties with a shared parking arrangement may be allowed to cross property lines and encroach the green space setback as required within the zoning district standards of the subject property. The elimination of the green space setback shall be only along the property line adjacent to the property to which a shared parking agreement is in place. The required green space square footage not provided along the property line due to the share parking agreement must be provided elsewhere on the subject site, in addition to the required green space of other property line setbacks.

Exhibit 23-2. Examples of land uses that could potentially share a parking area

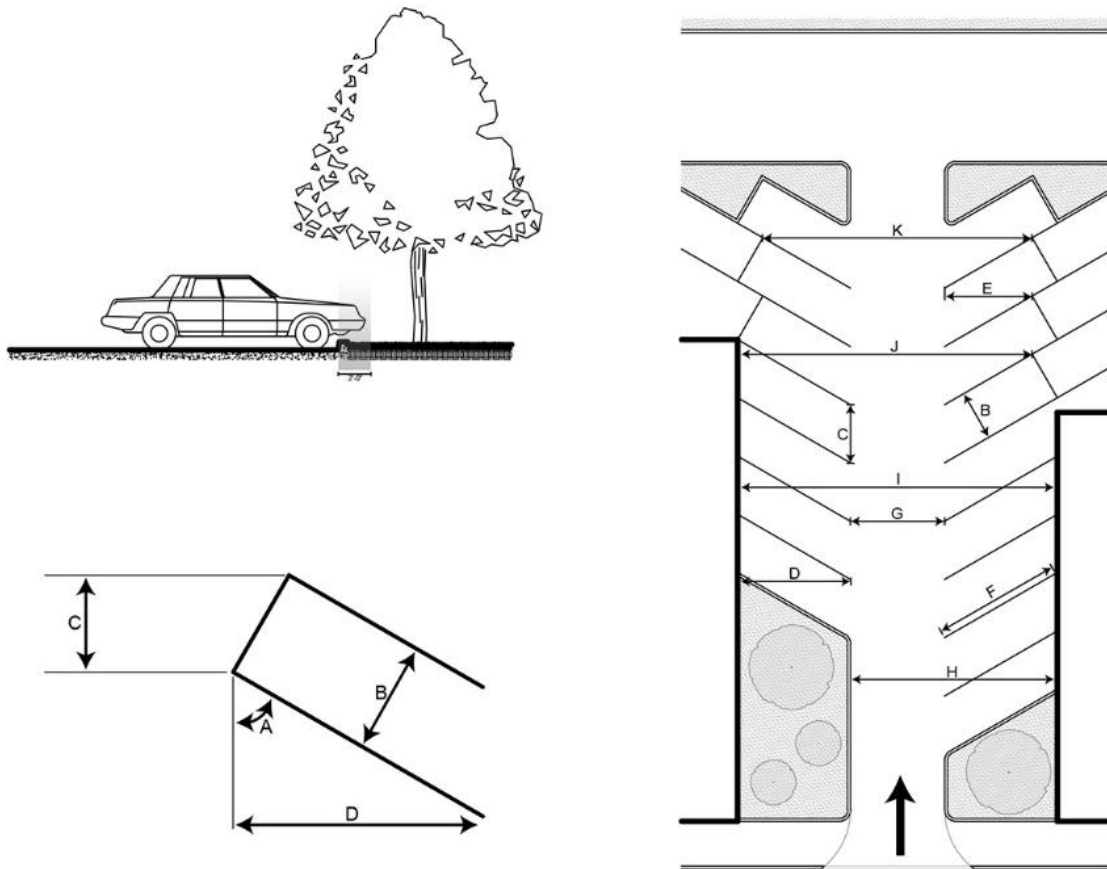
Land uses with typical weekday peaks	Land uses with typical evening peaks	Land uses with typical weekend peaks
Banks	Auditoriums	Religious institutions
Schools	Bars and dance halls	Parks
Distribution facilities	Meeting halls	Malls (some types, but not all)
Factories	Restaurants (some types, but not all)	
Medical clinics	Movie theaters	
Offices		
Professional services		

490-2809 Dimensional standards for parking areas

Parking spaces, except for accessible parking; access aisles; and other features in a parking area must conform to the dimensions in Exhibit 23-3. For parking angles not specified, the City Engineer is authorized to establish corresponding standards.

Exhibit 23-3. Parking area dimensional standards

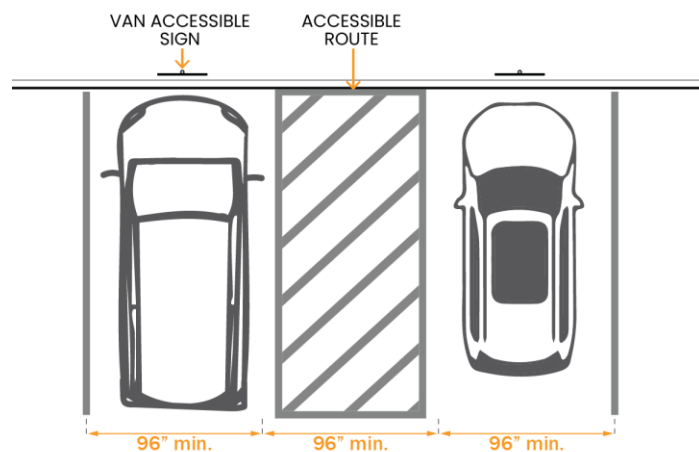
	Parking angle - A				
	0°	45°	60°	75°	90°
B Stall width at parking angle (feet)	9.0	9.0	9.0	9.0	9.0
C Stall width parallel to access aisle (feet)	17.0	12.7	10.4	9.3	9.0
D Stall depth to wall (feet)	9.0	17.5	19.0	19.5	18.5
E Stall depth to interlock (feet)	--	15.3	17.5	18.8	--
F Stall length (feet)	18.0	18.0	18.0	18.0	18.0
G Aisle width (feet)	12.0	12.0	16.0	17.2	24.0
H Module width – wall to wall (single-loaded) (feet)	21.0	29.5	35.0	42.5	44.5
I Module width – wall to wall (double-loaded) (feet)	30.0	47.0	54.0	62.0	63.0
J Module width – wall to interlock (double-loaded) (feet)	--	44.8	52.5	61.3	--
K Module width – interlock to interlock (double-loaded) (feet)	--	42.6	51.0	60.6	--



490-2810 Accessible parking spaces

- A. Generally.** Accessible parking spaces must be provided subject to this part; the Americans with Disability Act (ADA), as may be amended; and the *ADA Standards for Accessible Design* 28 CFR 36, revised as of July 1, 1994 as may be amended. In the event state and federal regulations are more controlling than the requirements in this section, the state and federal regulations control.
- B. Number required.** If parking spaces are required, then accessible spaces must be provided in the required number of regular spaces in the quantity shown in Table 23-1. One of 8 accessible parking spaces, but always at least one, must be van-accessible.

Exhibit 23-4. Layout of standard and van accessible parking spaces



C. Location. Accessible spaces serving a particular building must be located on the shortest accessible route of travel between the parking and the accessible entrance. When there are multiple entrances to a building of similar prominence (e.g., shopping mall) with near-by parking, accessible spaces must be dispersed and provided at each location. In parking facilities that do not serve a particular building, accessible parking must be located on the shortest accessible route of travel to an accessible pedestrian entrance of the parking facility.

D. Dimensions. Accessible parking spaces must be at least 96 inches wide.

E. Vertical clearance. For van-accessible parking spaces, a 98-inch high clearance must be maintained above the space, access aisle, and on the route to and from the van-accessible space.

F. Maximum slope. Accessible spaces and adjoining access aisles must have a maximum slope of 1:50 in all directions. When accessible spaces are provided in an existing parking lot, the spaces must be located on the most level surface close to the accessible building entrance.

G. Signage. Each accessible space must be so designated with a sign identified by the international symbol of accessibility mounted on a vertical pole. In addition, van-accessible spaces must be so designated with a sign indicating “Van Accessible.” Such signs must be located so they cannot be obscured by a vehicle parked in the space (at least 6 feet in height).

H. Pavement striping and markings. The boundary of the access aisle must be marked and the end of which may be squared or a semicircle. Additional pavement markings denoting the space are optional.

I. Accessible route. An accessible route must be provided from the accessible parking space to the accessible entrance of the building. It must be at least 36 inches wide, without steps or curbs. It must be paved and not contain any feature that would restrict, inhibit, or unreasonably impeded the movement of a physically disabled individual.

J. Access aisle. An access aisle for an accessible space must be the same length as the adjacent parking space it serves and be at least 60 inches wide for car access and 96 inches wide for van-accessibility. Two adjoining accessible parking spaces may share a common access aisle (Exhibit 23-3). An access aisle for a passenger loading zone must be 60 inches wide and 20 feet long and adjacent and parallel to the space.

Table 23-1. Minimum number of accessible parking spaces

Number of provided parking spaces	Number of accessible spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1000	2 percent of total
1,001 and over	20; plus 1 for each 100 over 1,000

490-2811 Electric vehicle charging stations

A parking area with 20 spaces or more must have one parking space that is EV capable (i.e., underground conduit is installed to accommodate a future charging station) for every 20 spaces.

490-2812 Pedestrian circulation routes and access

- A. General.** The site must have sidewalks and other pedestrian walkways to provide for the safe movement of pedestrians using the site.
- B. Perimeter sidewalk.** A 5-foot wide sidewalk must be provided on the perimeter when the vehicle parking spaces front on the front or side of the building (Exhibit 23-6).
- C. Pedestrian routes in a parking area.** When a pedestrian circulation route crosses a vehicular route, the Zoning Administrator may require a marked crosswalk to provide pedestrian safety (Exhibit 23-7).
- D. Connection to public sidewalk.** If a subject property fronts on a street with an existing or planned public sidewalk, a sidewalk at least 5 feet in width must connect the building entrance to the public sidewalk.
- E. Connection to bike trail and mid-block walkway.** If a subject property abuts a bike trail or mid-block walkway, a sidewalk at least 5 feet in width must connect the site to the bike trail or mid-block walkway.
- F. Public sidewalks.** When proposing a new project, the developer is responsible for installing a public sidewalk fronting the subject property when [redacted]. (verify with current practice if any)

Exhibit 23-5. EV-Capable parking spaces

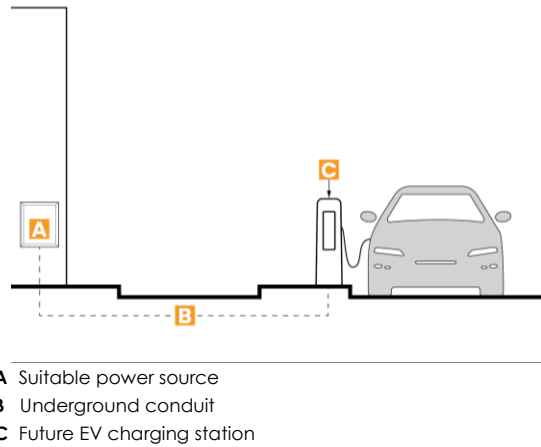


Exhibit 23-6. Sidewalk on perimeter of parking lot

Graphic forthcoming...

Exhibit 23-7. A pedestrian crosswalk in the parking lot of a fast-food restaurant



490-2813 Motorcycle parking

- A. Design.** A motorcycle parking space is at least 4 feet by 8 feet.
- B. Location.** Motorcycle parking spaces should be grouped together to the extent possible

490-2814 Bicycle parking

A. Legislative findings. The Common Council makes the following findings with regard to bicycle parking:

- (1) The provision of bicycle parking recognizes cycling as a transportation mode that is growing in popularity.
- (2) Designated bicycle parking areas help keep sidewalks clear for pedestrians and prevents damage to trees and street furniture often used to secure bicycles when racks are not present.
- (3) Properly designed bicycle parking areas enhance security and visibility, helping to prevent theft and vandalism of parked bicycles.
- (4) The provision of bicycle parking allows property owners to install fewer vehicle parking spaces, an important cost savings.

Exhibit 23-8. An example of a covered bicycle rack



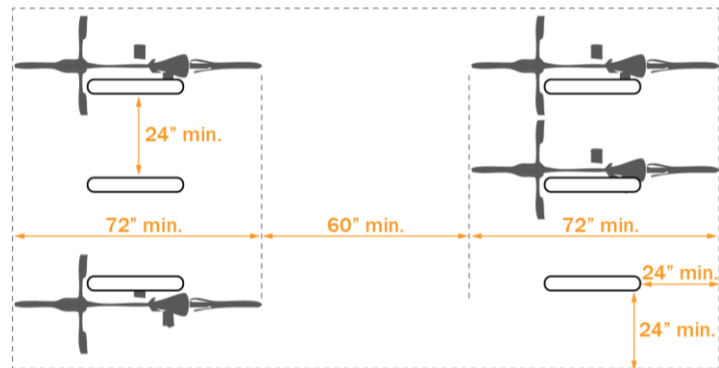
B. Generally. Bicycle parking may be provided consistent with the recommended standards in Appendix B.

Staff - Does the City want to require bicycle parking for some of the land uses? Alternatively, we could provide a recommended amount. By way of comparison, the City of Middleton has required standards for some land uses.

C. Location. Bicycle parking must be located in visible and prominent locations near the building entrance and must be as close or closer to the entrance than the nearest parking space. Under no circumstance should bicycle parking be more than 100 feet from the building entrance.

Where there is more than one building on a site, or where a building has more than one main entrance, the parking must be distributed to serve all buildings or main entrances. If possible, racks should be protected from the elements by a roof, awning, overhang, or similar covering. Racks must not be placed so they block the entrance or inhibit pedestrian flow in or out of the building.

Exhibit 23-9. Layout of bicycle parking spaces



D. Design. Bicycle parking areas should be incorporated into the overall building design, parking lot layout, and pedestrian circulation and coordinated with street furniture (e.g., benches, street lights, planters) when it is part of the overall project.

- E. Accessibility.** Each bicycle parking space must be accessible without moving another bicycle. In most circumstances, a space 2 feet by 6 feet is adequate. When needed, an aisle at least 5 feet wide must be provided (Exhibit 23-9) .
- F. Lighting.** Bicycle parking spaces must have adequate lighting to promote security and avoid vandalism and theft.
- G. Rack design.** Bicycle parking may be provided in ground, wall, or ceiling mounted racks. Ground-mounted racks should meet the following requirements:
 - (1) The rack holds the bicycle frame, not just a wheel.
 - (2) A U-shaped shackle lock can be used to secure the bicycle to the rack.
 - (3) The rack is designed to accommodate a wide range of bicycle sizes, wheel sizes, and types.
 - (4) The rack is covered with material that will not chip the paint off of a bicycle that leans against it.
 - (5) The rack does not have hazards, such as sharp edges.
 - (6) The rack is securely fastened to the ground, a wall, or other solid surface.

490-2815 Drive-throughs and stacking lanes

- A. Generally.** If allowed, a drive-through must meet the standards in this section and all other provisions that may apply.
- B. Overall design.** Stacking spaces, access lanes, and bypass lanes may not impede on- and off-street traffic movement or create hazards for pedestrians accessing public entrances of a building.
- C. Number of stacking spaces.** The minimum number of required stacking spaces, when allowed, is specified for various land uses in Appendix B.
- D. Standards for stacking spaces.** Stacking spaces must have a minimum depth of 20 feet.
- E. Standards for access lanes.** Access lanes and stacking spaces must have a minimum width of 12 feet (curb to curb).
- F. Standards for bypass lanes.** Any drive-through that has more than a total of 4 stacking spaces, must include a bypass lane with a minimum width of 10 feet (curb to curb).
- G. Markings required.** Access lanes and bypass lanes must be striped, marked, or otherwise distinctly delineated.
- H. Crosswalks.** A pedestrian crosswalk must be marked on the pavement when the lane for a drive-through window is situated between on-site parking and a building entrance.

490-2816 Loading berths (verify with Bob if needed)

- A. Applicability.** Off-street loading berths are required when so indicated in Appendix B.
- B. Minimum number of loading berths.** Those buildings subject to this section must provide one or more loading berths as specified in Table 23-2.
- C. Dimensional standards.** A loading berth must comply with the dimensional standards in Table 23-3. The minimum vertical clearance also applies to all areas providing access to the loading berth.
- D. Location.** A loading berth must not be located on the front of the building, except when entirely located inside of the building and the access door is integrated into the overall design of the building. A loading berth must not be located within a required side yard setback area. A loading berth must not be located within a public road right-of-way or interfere with the intended use of a public road right-of-way. A loading berth or access to a loading berth must not interfere with onsite traffic or pedestrian circulation or on-site parking.
- E. Surfacing.** A loading berth must be hard surfaced (e.g., concrete, asphaltic concrete, pavers, or similar product). If it is not possible to hard surface a loading berth between November 1 and April 1, the Building Inspector may issue an occupancy permit provided the property owner and Zoning Administrator enter into a written agreement that requires completion no later than June 1.
- F. Marking.** A loading berth must be clearly marked.
- G. Use.** A loading berth must only be used for loading and unloading of vehicles.
- H. Drainage.** A loading berth must be properly graded for drainage consistent with standards adopted by the City, and as approved by the city engineer.
- I. Outdoor lighting.** Outdoor lighting for a loading berth must comply with the requirements in Article 24.
- J. Screening.** The Plan Commission may require screening (e.g., landscaping, berm, fence, or any combination) when the use of the loading berth has the potential of negatively impacting adjoining residential uses.

Table 23-2. Minimum number of loading berths

Floor area	Minimum number of loading berths
6,000 to 12,499 square feet	1 small berth
12,500 to 24,999 square feet	2 small berths
25,000 to 39,999 square feet	1 large berth
40,000 to 99,999 square feet	2 large berths
100,000 square feet or more	2 large berths plus 1 large berth for each additional 80,000 square feet over 100,000 square feet

Table 23-3. Dimensions of loading berths by type

Type	Minimum width	Minimum length	Minimum overhead clearance
Small berth	10 feet	25 feet	14 feet
Large berth	12 feet	50 feet	14 feet

490-2817 Driveways

- A. Generally.** Driveways that are installed, altered, changed, replaced, or extended must meet the standards in this section and other applicable parts of this code and follow the review procedures in Article 5.
- B. Driveway setbacks.** A driveway must be at least 3 feet from a side lot line and a rear lot line. This setback standard does apply to a shared driveway. If an existing driveway is located closer than 3 feet, it may be reconstructed in the same location
- C. Dimensional standards.** Driveways must comply with the standards in Table 23-4 as applicable.

Table 23-4. Driveway standards

Land use	Maximum width at lot line	Maximum width at street curb line	Number of access points
One- and two-family residential, townhouse	20 feet; 24 feet for shared driveway	Width at lot line plus 5 feet	One by right; two with Plan Commission approval, provided there is a minimum spacing of not less than 100 feet between the two driveways
Multi-family and manufactured home park	24 feet, plus 6 feet for boulevard, plus 12 feet for a turn lane supported by a traffic study	Width of lot line plus 6 feet	Two by right; three with Plan Commission approval, provided there is a minimum spacing of not less than 200 feet between the two driveways
Institutional, business, and commercial	24 feet, plus 6 feet for boulevard, plus 12 feet for a turn lane supported by a traffic study	Width of lot line plus 6 feet	Two by right; three or more with Plan Commission approval, provided there is a minimum spacing of not less than 100 feet between the two driveways
Industrial and warehousing	32 feet, plus 6 feet for boulevard, plus 12 feet for a turn lane supported by a traffic study	Width at lot line plus 6 feet	Two by right; three or more with Plan Commission approval, provided there is a minimum spacing of not less than 100 feet between the two driveways

D. Minimum width of internal access lanes. Internal access aisles/lanes must comply with the minimum widths as specified in Table 23-5.

Table 23-5. Minimum width of internal access lanes

Land use	One-way	Two-way
Multi-family, 3 to 6 units	10 feet	12 feet
Multi-family, 7 to 15 units	10 feet	20 feet
Multi-family, 16 or more units	12 feet	24 feet
Manufactured home park, less than 20 spaces	10 feet	20 feet
Manufactured home park, 20 spaces or more	12 feet	24 feet
Institutional, business, and commercial	12 feet	24 feet
Industrial and warehousing	12 feet	24 feet

E. Construction specifications. Driveway approaches that connect to a City street must be constructed in accordance with the specifications established by the City Engineer. Driveway approaches connecting to a county or state highway must comply with the standards established by the respective jurisdiction.

490-2818 Turning radius

In addition to all of the specific design requirements described in this article, loading berths, driveways, and access aisles must be designed to accommodate a turning template for a WB-50 truck which represents the turning radius of the City's largest fire truck.... or the City's largest fire truck as determined by the fire department. (verify with staff)

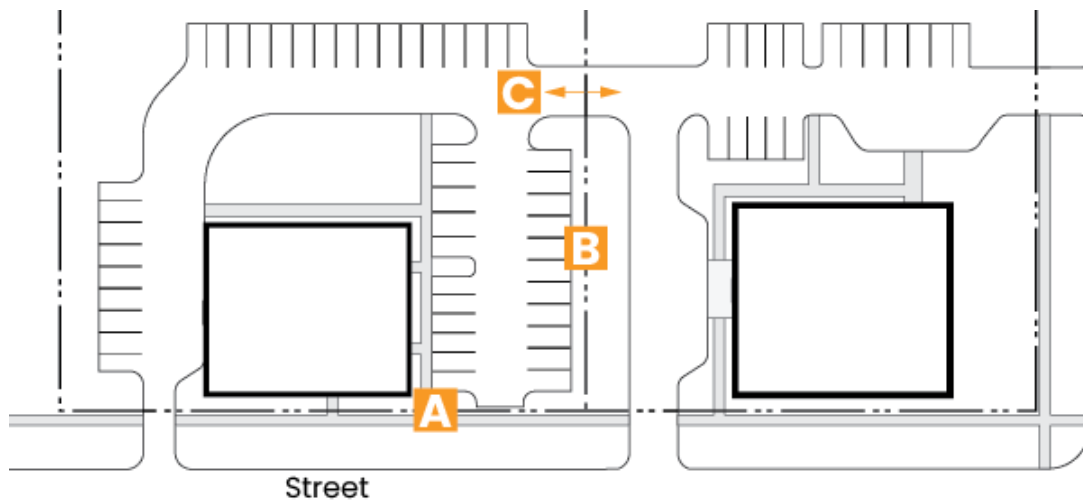
490-2819 Cross-access connection

A. Findings. A cross-access connection between two adjoining properties uses can improve traffic flow, safety, and land use efficiency as more fully described below.

- (1) By allowing vehicles to move between properties without re-entering public roads, cross-access connections help decrease congestion on major streets.

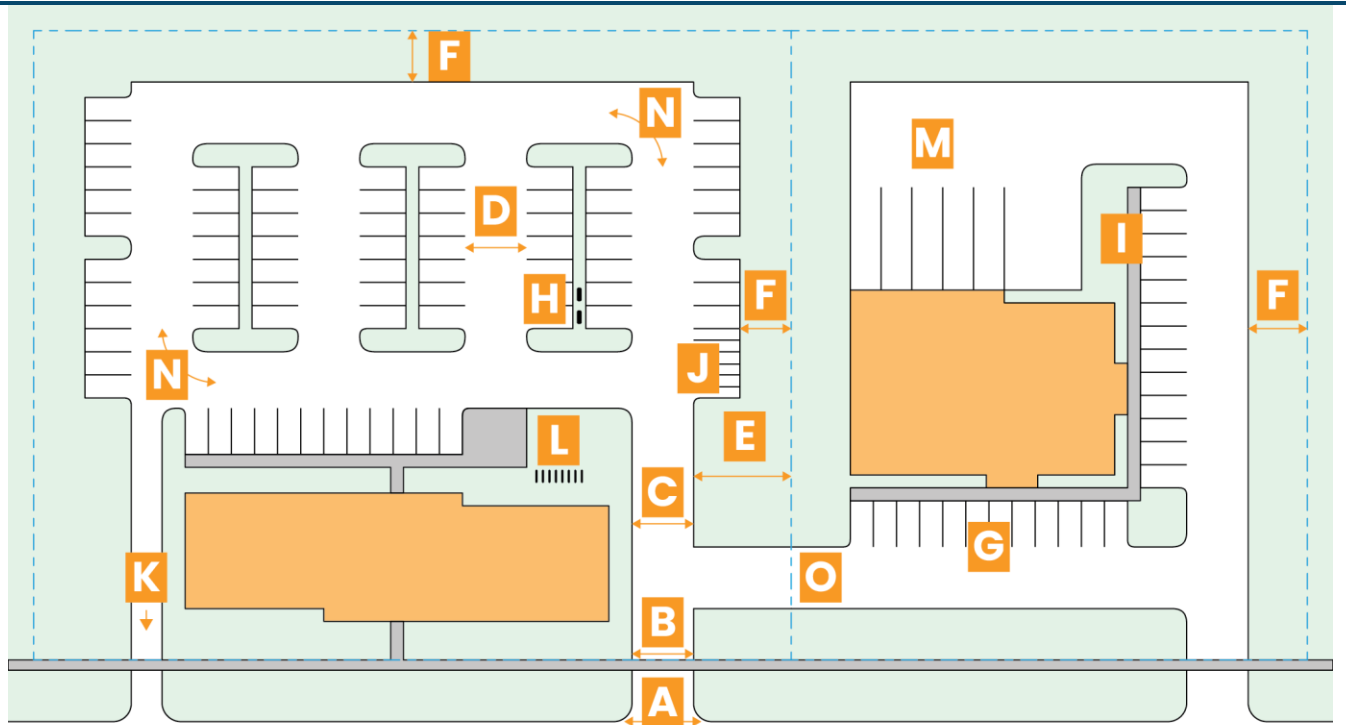
- (2) Fewer vehicles making left turns or entering/exiting from multiple driveways reduces the potential for crashes.
 - (3) Fire trucks, ambulances, and police will have multiple access routes to respond more quickly to emergencies.
 - (4) Businesses can potentially share parking lots, reducing the need for excessive pavement and improving land efficiency.
 - (5) Shoppers can easily access multiple businesses without having to make multiple stops onto a busy street.
 - (6) Cross-access connections can facilitate pedestrian and bicycle access between properties, encouraging multimodal transportation.
- B. Applicability.** A cross-access connection is required when both of the adjoining properties allow commercial and mixed-uses and are located on an arterial street OR a major street as depicted on the adopted zoning map (verify). Upon a written petition, the Zoning Administrator may waive this requirement if making the cross-connection is not feasible because of an environmental constraint or would create unsafe traffic movements.
- C. Easement.** The adjoining properties must be subject to a cross-access easement agreement which is recorded in the register of deeds office for Ozaukee County.

Exhibit 23-10. Cross-access connection



- Key:**
- A Front lot line
 - B Side lot line
 - C Cross-access connection

Exhibit 23-11. Composite layout of parking and access lanes and related features



- | | |
|--|---|
| <p>A Maximum width at street curb line. See Table 23-4</p> <p>B Maximum width at lot line. See Table 23-4</p> <p>C Minimum width of internal access lanes. See Table 23-5</p> <p>D Minimum width of internal parking lot aisle. See Exhibit 23-2</p> <p>E Minimum setback for access lanes. See [redacted]</p> <p>F Minimum setback from side and rear lot lines. See [redacted]</p> <p>G The number of parking spaces in the front yard may be restricted. See § 490-[redacted] and Appendix C</p> | <p>H EV-ready parking spaces. See § 490-[redacted]</p> <p>I Internal sidewalks. See § 490-[redacted]</p> <p>J Motorcycle parking. See § 490-[redacted]</p> <p>K Drive-through with stacking lanes. See § 490-[redacted]</p> <p>L Bicycle parking. See § 490-[redacted]</p> <p>M Loading bays. See § 490-[redacted]</p> <p>N Turning radius. See § 490-[redacted]</p> <p>O Cross-access connection. See § 490-[redacted]</p> |
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490-2820 Access gate

- A. Setback.** The gate must be at least 28 feet from the edge of the road, but no closer than 8 feet to the front lot line. The gate, in any position, may not be any closer than 8 feet to any lot line.
- B. Width of gate.** The width of the gate must be at least 12 feet but not more than 24 feet.
- C. Emergency access.** The gate must include a Knox Box or an emergency override system as approved by the local fire department.
- D. Nighttime visibility.** The gate must include reflective strips or signage for nighttime visibility. Outdoor lighting may also be used for this purpose.
- E. Power.** If a gated access drive includes an electrically powered gate, the power supply for the gate must be on a separate metered service (i.e., independent of the principal building) to ensure continuous operation in the event of a building power failure.

490-2821 through 490-2899 reserved